

Carey Mack Weber

Mark W. Alexander  
Transportation Assistant Planning Director  
CTDOT, Bureau of Policy and Planning  
2800 Berlin Turnpike, Newington, CT 06131

June 22, 2016

Dear Mr. Alexander,

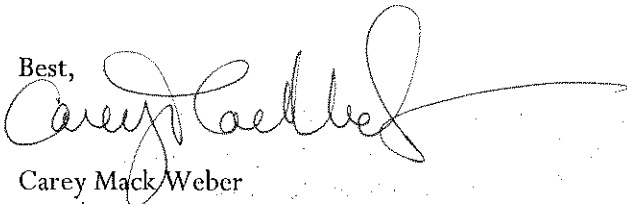
I am a 22 year resident of Bridge Street living in a Victorian house that was built in 1877, three houses from the Cribari Bridge. I will be impacted on a daily basis by whatever decision is made about the Bridge by your department. I attended the most recent Planning and Scoping meeting at the Westport Town Hall and would like to register my objection to both of your proposed plans for the bridge. I do not want a new bridge and I am not in favor of the rehabilitation plan that you outlined at the meeting. I am not in favor of any plan that would allow 18 wheel truck/tractor trailers to use the bridge. Heightening the bridge which was a feature in both of your plans would allow and encourage trucks to use Bridge Street and the Bridge as an alternate route to 95. This would be untenable.

A seven day a week traffic study MUST be done, during the school year. You will find that the traffic comes to a halt on Bridge Street with idling traffic for an hour at a time, at various times of day - most mornings between 8-9, Friday evenings when the crowds descend on Saugatuck restaurants and are looking for parking, and at other times when the traffic on 95 is backed up... The diesel exhaust and noise from trucks would drastically diminish our quality of life in an historic neighborhood. The safety issues relating to large trucks on this route with five bus stops is also a major issue. I think you will also find that at rush hour there is so much congestion at the intersections at Riverside and 136 (near the train station/Tutti's) and Riverside/Treadwell/Saugatuck that even if you had a better turning lane coming off the bridge into Saugatuck the cars would get stopped up at their next light and it would still get backed up down Bridge Street and Greens Farms Road. The traffic often goes back more than a mile down Green Farms Road. As you heard at the meeting those of us who live in this neighborhood feel strongly that we do not want to make this alternate route more appealing to drivers of cars, and certainly not to truck drivers.

An additional thought - I am in favor of raising the pilings (by a few feet) on the bridge when the underpinings/piers are restored, if this is an option, both to raise the mechanicals out of the way of storm surges, and to allow for some additional boat traffic. Then when the historic upper structure is restored it can just be placed back on top of this slightly higher sub-structure. Obviously, I am not an engineer, so this may not be feasible, but I thought I would throw it out there.

Thank you for your consideration of my comments. The takeaway here is that I want the existing bridge repaired and kept as is. My other idea is just to add to the mix of community input.

Best,



Carey Mack Weber