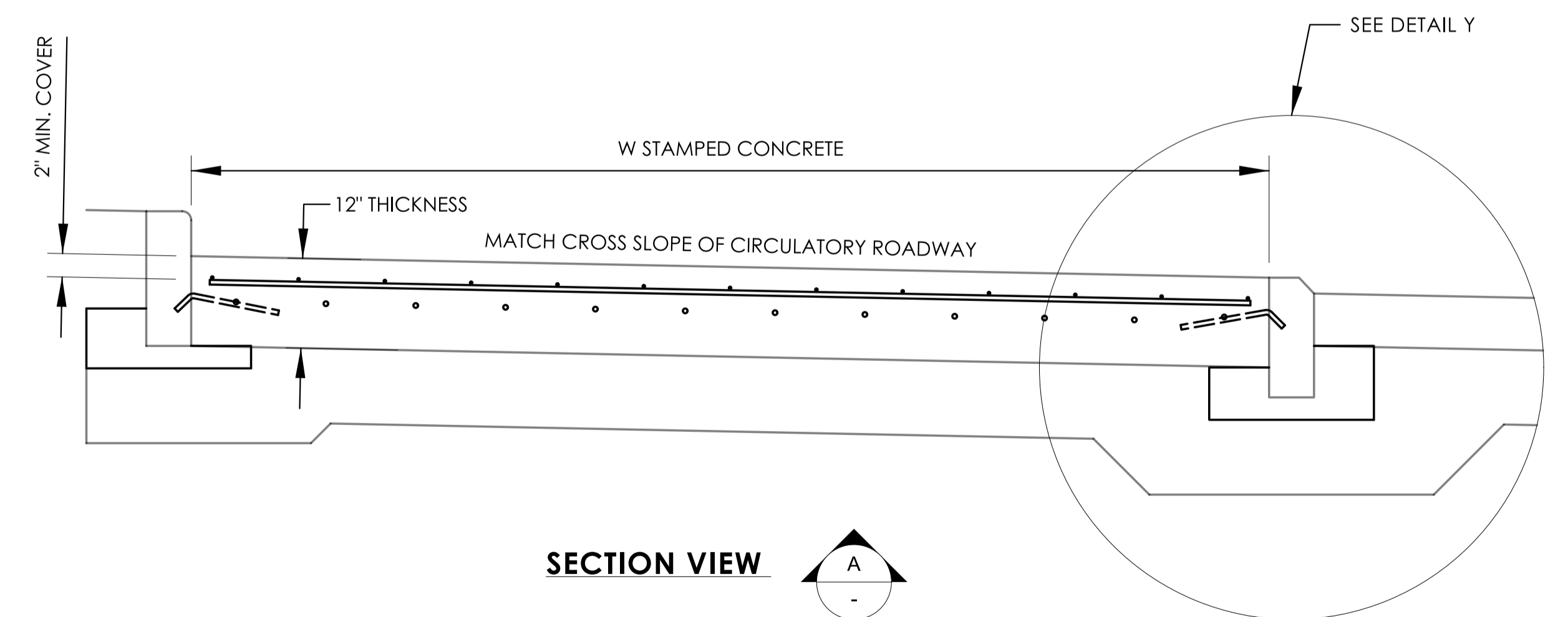
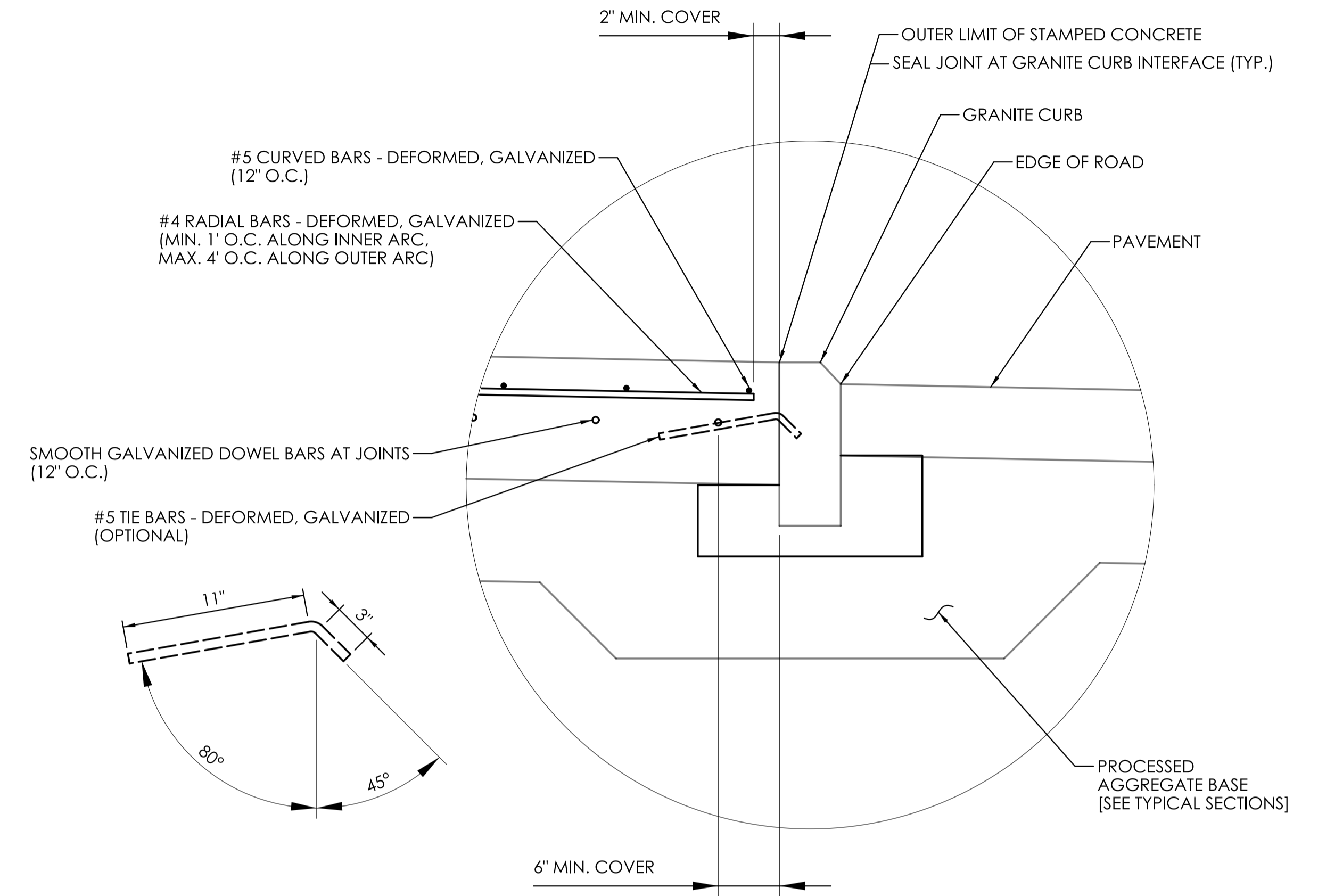
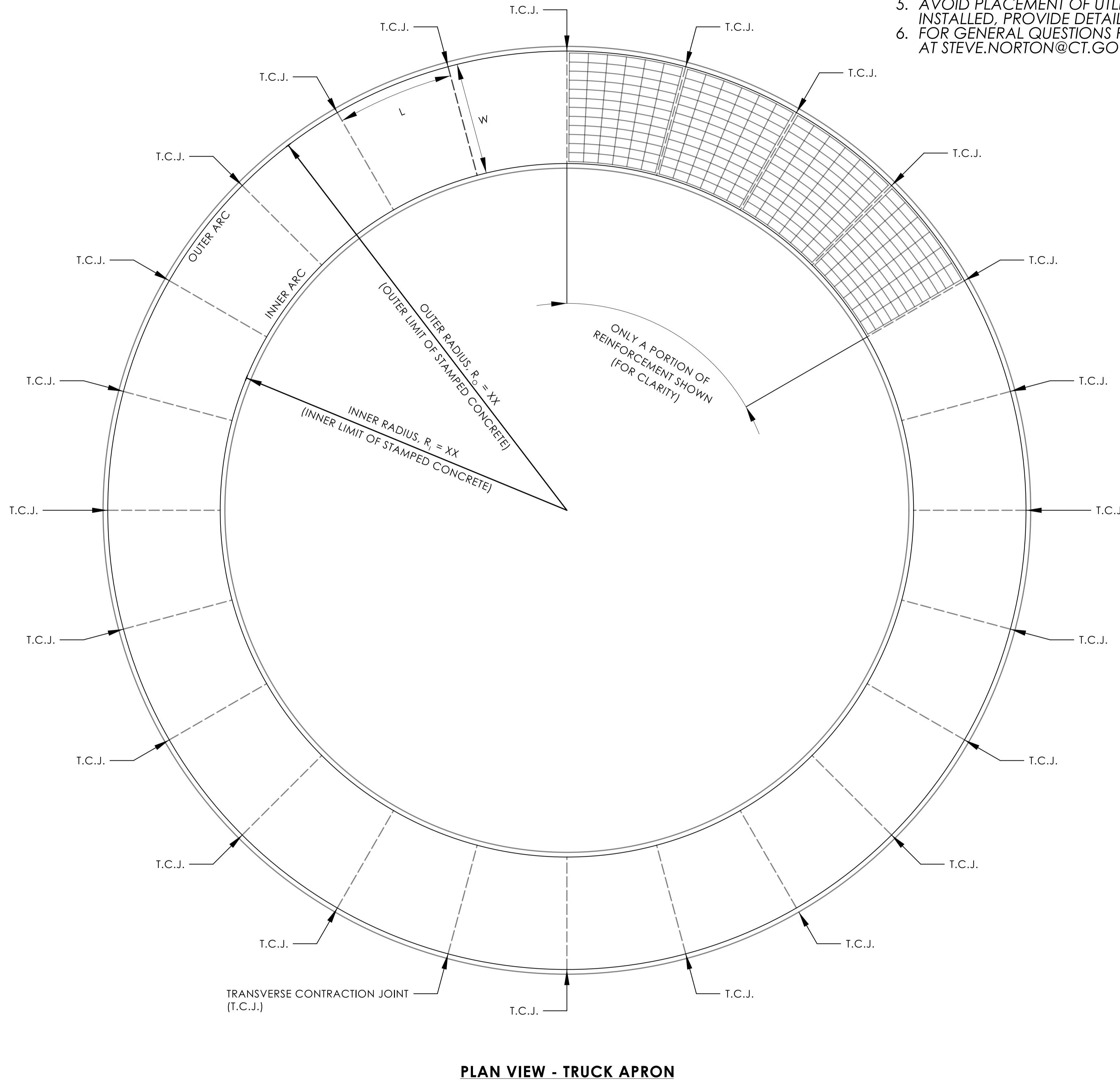


**GENERAL NOTE**

1. "O.C." - ON CENTER.

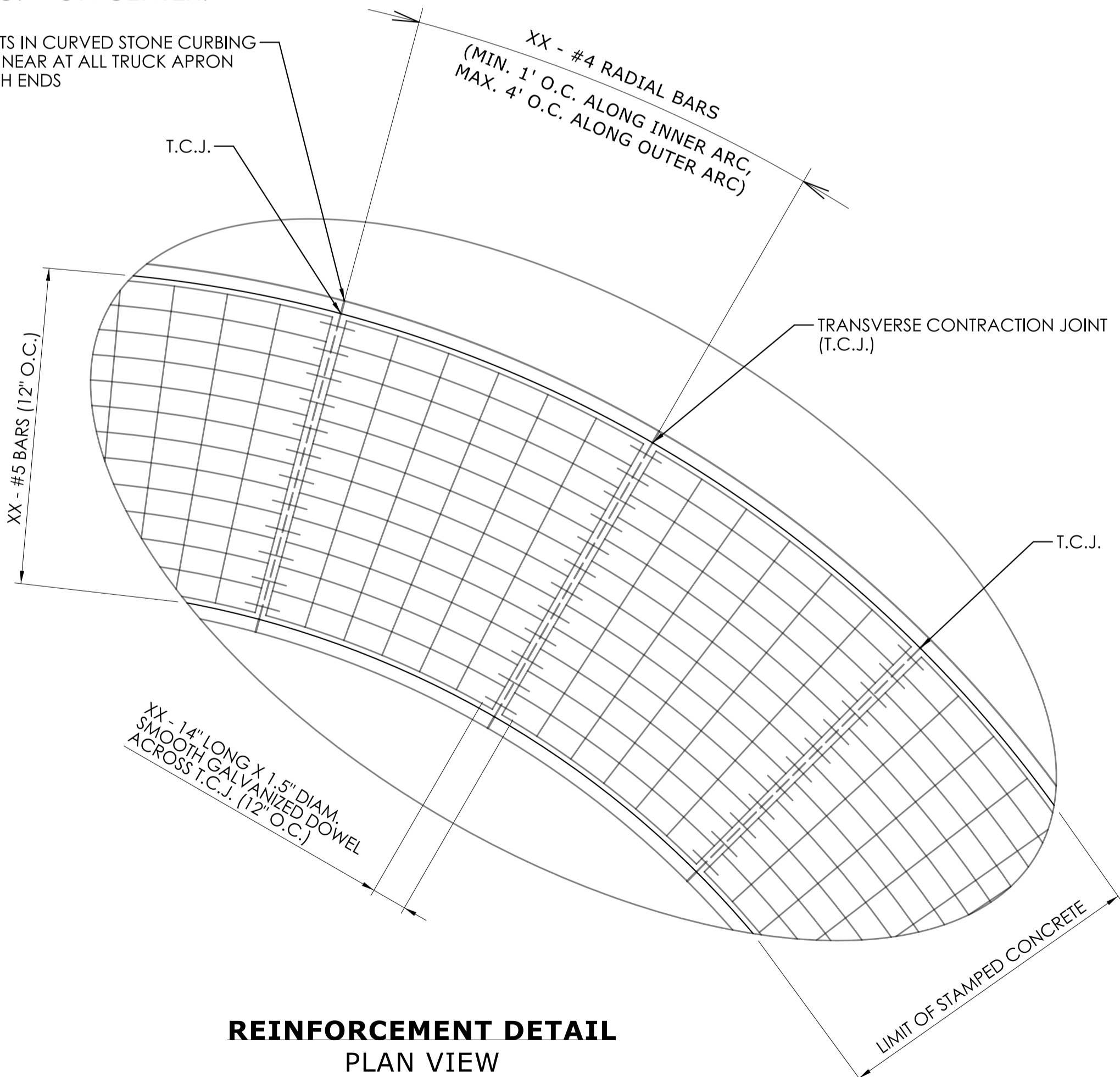
- >> NOTES FOR DESIGNER - TO BE REMOVED FROM FINAL SHEET:
2. LENGTH (L) AND WIDTH (W) DIMENSIONS OF TRUCK APRON SLAB SHALL BE 15' (MAX.) WITH A RATIO OF L:W OR W:L NO GREATER THAN 1:1.5, WITH 1:1 BEING IDEAL.
  3. SIZE AND SHAPE OF TRUCK APRON MAY VARY. "XX" DENOTES VALUE DEPENDENT ON DESIGN OF TRUCK APRON.
  4. TWO LAYERS OF REINFORCEMENT (AN ADDITIONAL LAYER PLACED IN THE BOTTOM 1/3 OF SLAB) MAY BE CONSIDERED WHERE THERE IS AN EXPECTATION OF LOSS OF SUPPORT BASED ON EXISTING CONDITIONS, SUCH AS HIGH GROUNDWATER TABLE OR POOR DRAINAGE.
  5. AVOID PLACEMENT OF UTILITY MANHOLE, GATES, AND OTHER STRUCTURES WITHIN CONCRETE TRUCK APRON. IF UTILITY ACCESS MUST BE INSTALLED, PROVIDE DETAILS FOR JOINTS AROUND UTILITY.
  6. FOR GENERAL QUESTIONS REGARDING TRUCK APRON GUIDANCE SHEETS, CONTACT STEVE NORTON, PAVEMENT DESIGN UNIT TSE, AT STEVE.NORTON@CT.GOV.



**GENERAL NOTES**

- MATERIALS FOR SIDE FRAMES AND CENTER SUPPORTS SHALL MEET THE REQUIREMENTS OF STANDARD SPECIFICATIONS M.06 - METALS. ALTERNATE FRAME DESIGN MAY BE APPROVED.
- ALL SIDE FRAMES AND CENTER SUPPORTS SHALL BE 1 GAUGE WIRE OR 5/16" BARS THROUGHOUT.
- SUPPORT REINFORCING BARS WITH CONCRETE BLOCKS OF THE SAME STRENGTH AS CONCRETE FOR PAVEMENT.
- CONCRETE SHALL ADHERE TO ARTICLE M.03.02, CLASS PCC03540.
- ALL REINFORCEMENT, DOWEL BARS, AND BASKETS SHALL BE GALVANIZED ACCORDING TO ASTM A767, CLASS 1. DOWEL BARS SHALL MEET ASTM A615, GRADE 60 SPECIFICATIONS. DOWEL BARS AND BASKETS TO BE PAID UNDER ITEM #0401201 "TRANSVERSE CONTRACTION JOINT". ALL OTHER ELEMENTS TO BE PAID UNDER ITEM #0401101 "MAT REINFORCEMENT FOR CONCRETE PAVEMENT", UNLESS OTHERWISE NOTED.
- DOWEL BAR DIAMETER SIZES ARE EXCLUSIVE OF COATINGS. ALL DIMENSIONS SUBJECT TO MANUFACTURING TOLERANCES.
- BOND-BREAKING MATERIAL APPLIED TO EACH DOWEL SHALL CONSIST OF PARAFFIN WAX, LITHIUM GREASE, OR OTHER SEMI-SOLID, INERT LUBRICANT.
- CONTRACTION JOINTS SHALL HAVE JOINT SEALANT APPLIED ACCORDING TO SUBARTICLE M.03.08-5. AT FIRST POURING, THE JOINT SEAL MATERIAL SHALL FILL THE JOINT FLUSH WITH THE PAVEMENT SURFACE. AFTER THIS MATERIAL HAS COOLED AND CONTRACTED, THE REMAINING JOINT OPENING SHALL BE FILLED FLUSH WITH THE PAVEMENT SURFACE.
- INTERFACE BETWEEN GRANITE CURBING AND CONCRETE PAVEMENT SHALL HAVE JOINT SEALANT APPLIED ACCORDING TO SUBARTICLE M.03.08-5 AND SHALL BE INCLUDED IN THE COST OF ITEM NO. XXXXXXX.
- "O.C." - ON CENTER.

MORTAR JOINTS IN CURVED STONE CURBING SHALL BE COLINEAR AT ALL TRUCK APRON JOINTS AT BOTH ENDS

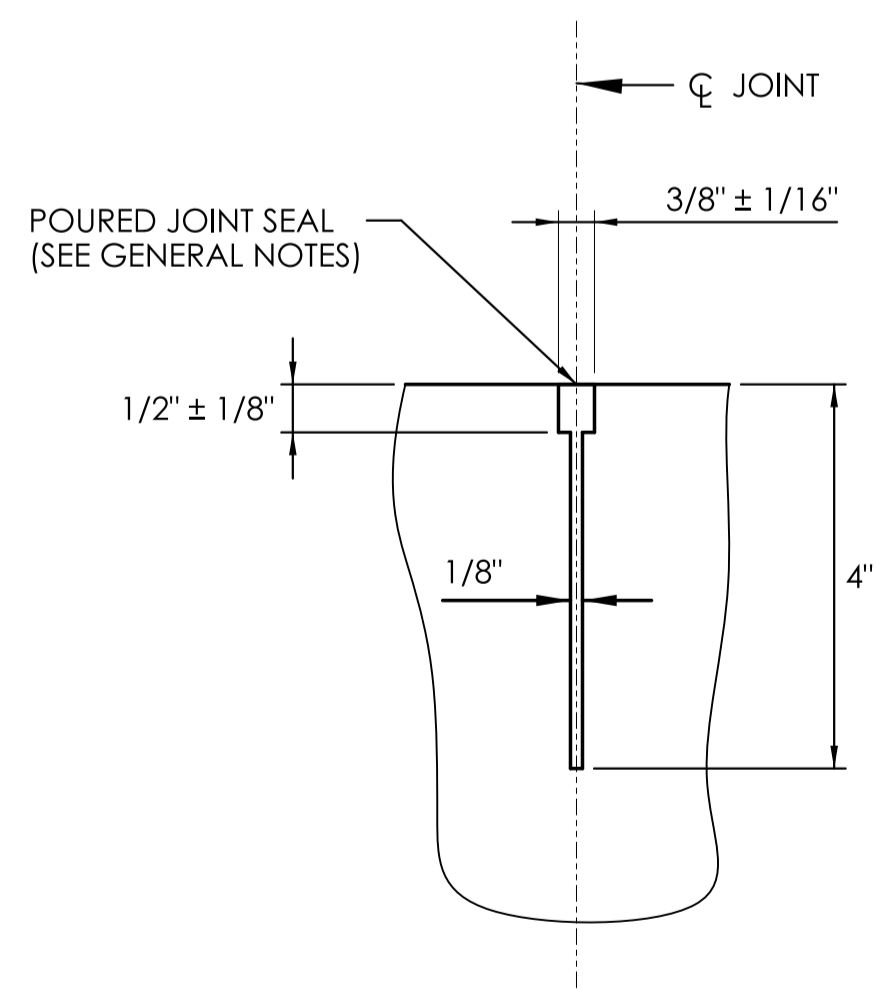
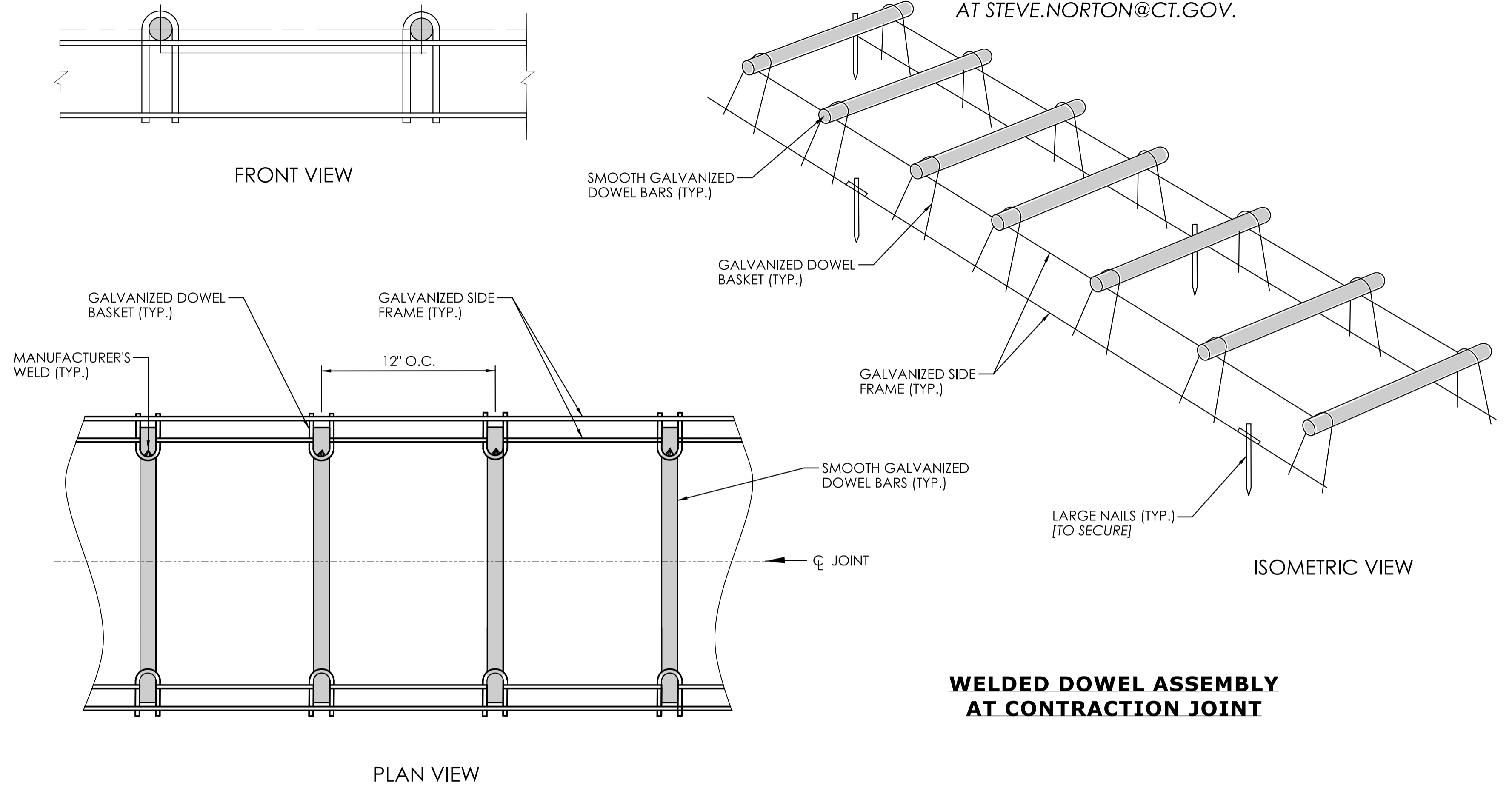


**REINFORCEMENT DETAIL  
PLAN VIEW**

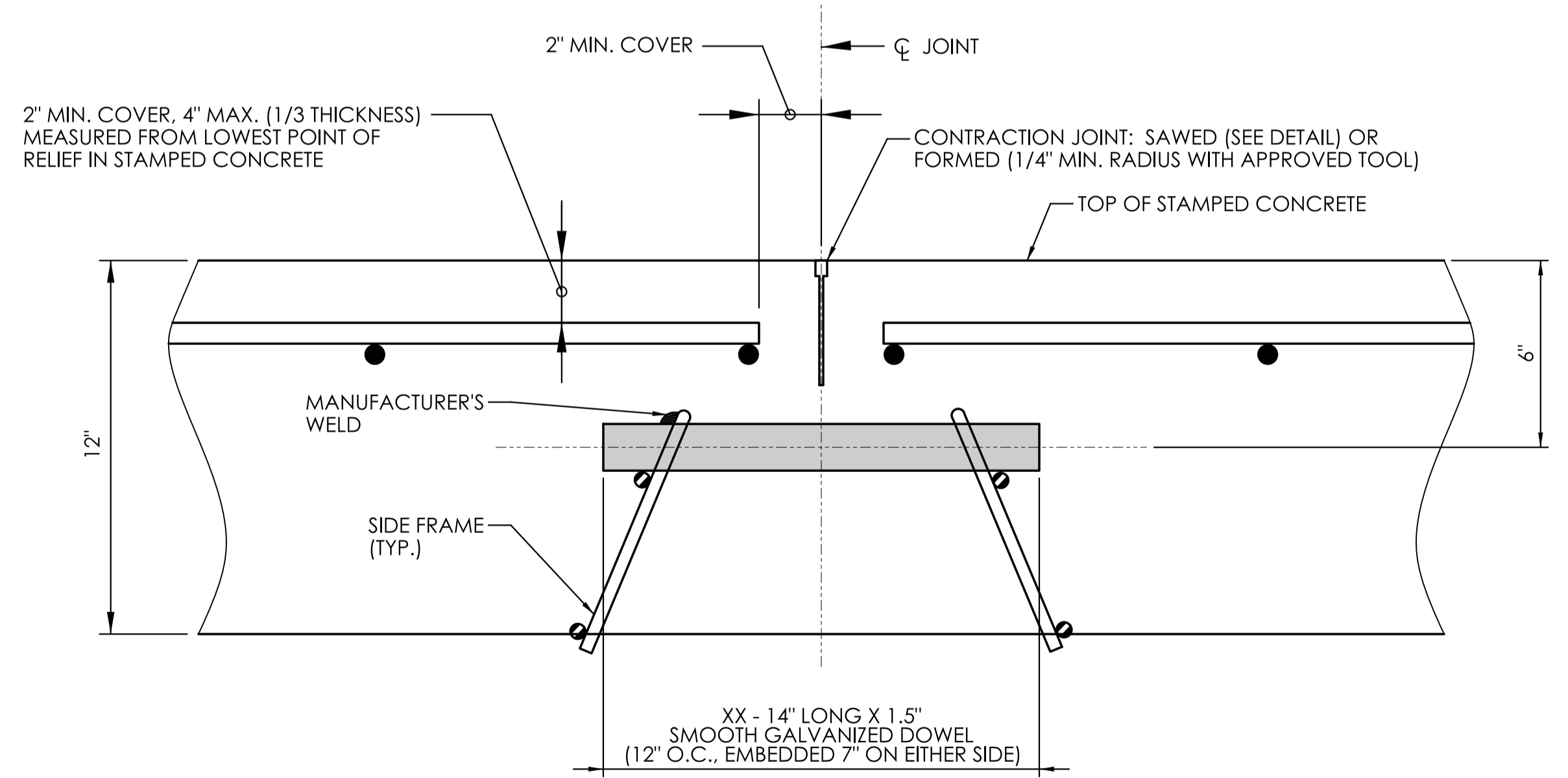
>> NOTES FOR DESIGNER - TO BE REMOVED FROM FINAL SHEET:

- SIZE AND SHAPE OF TRUCK APRON MAY VARY. "XX" DENOTES VALUE DEPENDENT ON DESIGN OF TRUCK APRON.
- GENERAL NOTE #9: JOINT SEALANT AT CURB/CONCRETE INTERFACE SHALL BE PAID FOR UNDER SAME ITEM USED TO PAY FOR GENERAL CONCRETE WORK (I.E., 0401000 "CONCRETE FOR PAVEMENT" OR 0601020A "STAMPED CONCRETE", WHICHEVER IS USED IN CONTRACT). FOR "SECTION 4.01 - CONCRETE FOR PAVEMENT" SPECIAL PROVISION, CONTACT STEVE NORTON, PAVEMENT DESIGN UNIT TSE, AT STEVE.NORTON@CT.GOV.

■ DENOTES BOND BREAKER APPLIED OVER DOWEL BAR.



**SAWED CONTRACTION JOINT DETAIL**



**TRANSVERSE CONTRACTION JOINT SECTION VIEW**