



Description

- There are 3,719 centerline miles of state-maintained routes and roads in Connecticut, 1,405 of which are on the National Highway System (NHS), including 346 Interstate miles.
- There are another 17,419 centerline miles of town maintained roads, 56 of which are on the NHS.
- 70.9% of CTDOT maintained roadways are flexible (asphalt) pavements, 28.7% are composite pavements (asphalt over concrete), and 0.4% are rigid (concrete) pavements.

State of Good Repair (SOGR)

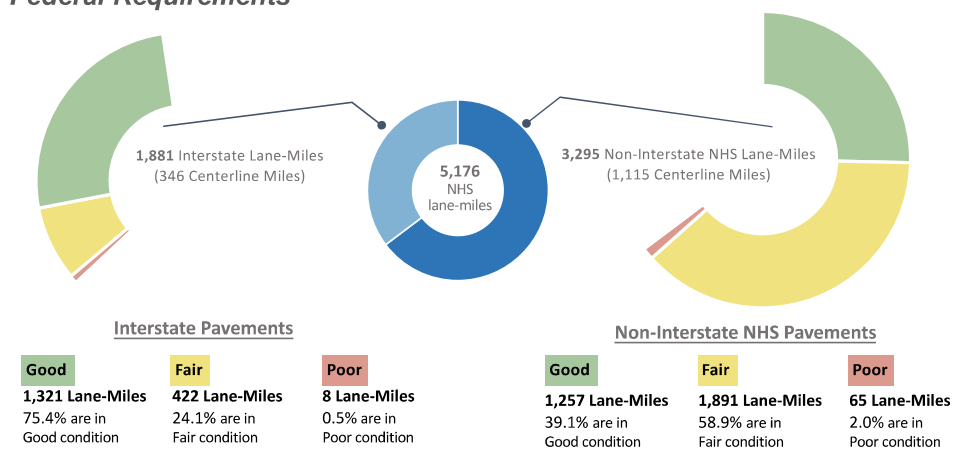
A pavement section for which the Pavement Condition Index (PCI) is 6 or greater is classified as being in a State of Good Repair (SOGR). The PCI is based on cracking, rutting, drainage disintegration, and ride. FHWA uses different condition measures for NHS pavements.

Pavement Age

The average Connecticut NHS pavement structure was constructed 44.8 years ago, and the average surface age is 7.8 years old.

NHS Roadways Inventory and Condition

Federal Requirements



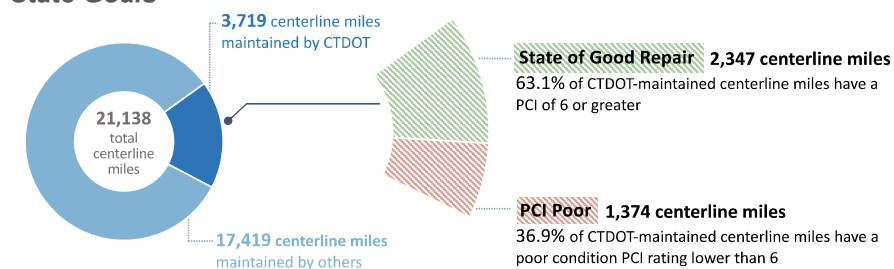
Note on Interstate: Total condition lane miles of 1,751 excludes 130 lane miles coded as bridge and zero lane miles missing/invalid.
 Note on Non-Interstate NHS: Total condition lane miles of 3,213 excludes 81 lane miles coded as bridge and 1 lane mile missing/invalid. Totals include 159 NHS lane miles which are locally maintained, 91.8% in fair condition and 8.2% in poor condition.

Based on CTDOT 6/15/19 HPMS Submittal

Good-Fair-Poor defined by MAP-21/FAST Act

CTDOT-Maintained Roadways Inventory and Condition

State Goals

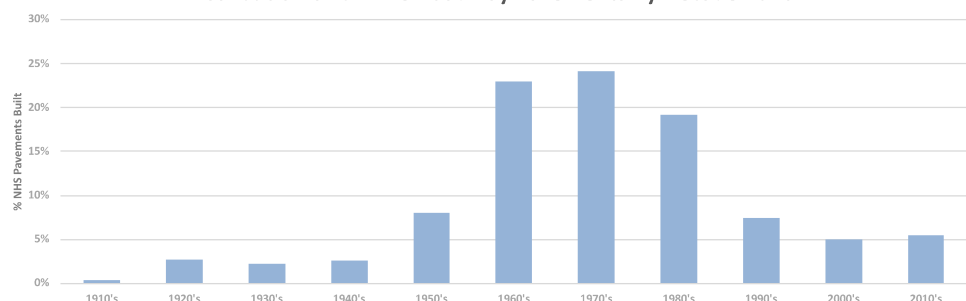


Based on CTDOT 6/15/19 Snapshot

SOGR defined by CTDOT

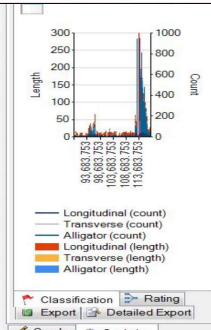
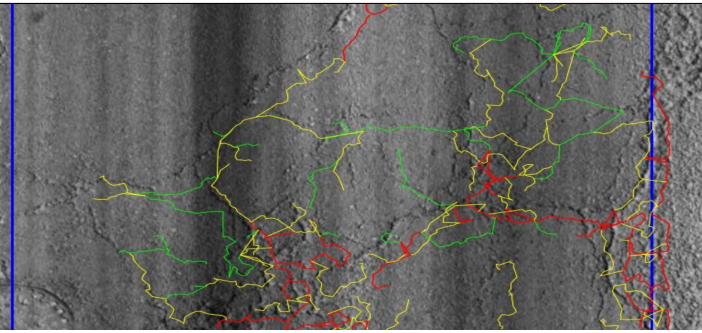
History

Distribution of CT NHS Roadway Pavements By Decade Built



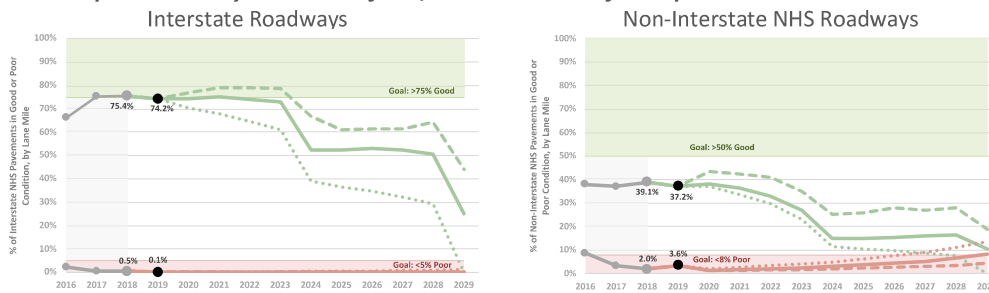


Connecticut Transportation Asset Management Plan Pavement



NHS Pavement Performance Projections

Federal Requirements by lane miles for 4,964 lane miles of NHS pavement



% Good (by lane miles)

— Preferred Funding (\$450M)
 — Current Funding (\$130M)
 ⋯ No Funding (\$0M)
 ● Actual Conditions

% Poor (by lane miles)

— Preferred Funding (\$450M)
 — Current Funding (\$130M)
 ⋯ No Funding (\$0M)
 ● Actual Conditions

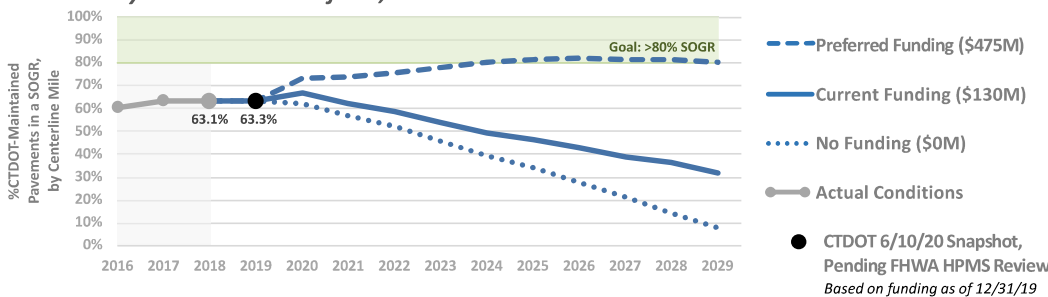
Based on funding as of 12/31/19

Performance Projections at Current Funding Level (\$130M Budget)

End of Year	2020	2021	2022	2023	2024	Goal
Interstate Good	74.2%	75.1%	73.9%	72.8%	52.2%	75.0%
Interstate Poor	0.1%	0.1%	0.1%	0.1%	0.1%	<5.0%
Non-Int NHS Good	38.1%	36.4%	33.1%	27.0%	15.2%	50.0%
Non-Int NHS Poor	1.5%	1.7%	2.1%	2.4%	3.0%	<8.0%

CTDOT-Maintained Pavement Performance Projections

State Goals by centerline miles for 3,719 centerline miles



Performance Projections at Current Funding Level (\$130M Budget)

End of Year	2020	2021	2022	2023	2024	Goal
SOGR	66.7%	62.3%	58.7%	53.9%	49.4%	80.0%

Performance Projections

The charts on the left depicts pavement condition for various funding scenarios. These were developed through an analysis program using CTDOT pavement deterioration curves as of 2019.

Asset Valuation

\$10,972,500,000

Asset value is estimated using the replacement value. For pavements, replacement value is the product of pavement area (SY) and unit construction cost. For 3,719 centerline miles of pavement: 104.5 million SY * \$105/SY = \$10.97 Billion

Measures and Targets

CTDOT has set the following pavement condition goals:

Federal Requirements:

- Interstate: 75% good condition and less than 5% poor condition (Federal minimum is less than 5% poor)
- Non-Interstate: 50% good condition and less than 8% poor condition

State Goal:

- 80% or more of State-maintained pavements in a SOGR (State)