

MEETING MINUTES

STATE PROJECT NO.: 126-176

DATE OF MEETING: January 13, 2022 (7:00 PM to 8:00 PM)

LOCATION OF MEETING: Microsoft Teams

SUBJECT OF MEETING: Public Information Meeting

PROJECT NAME: Rehabilitation of Bridge No. 00571A (Commodore Hull Bridge)
Route 8 over the Housatonic River - Shelton/Derby, CT

ATTENDANCE LIST:

Name	Organization/Agency	Phone	Email
Michelle Lynch	CTDOT	(860) 594-3206	Michelle.Lynch@ct.gov
Jessica Carvajal	CTDOT	(860) 594-2774	jessica.carvajal@ct.gov
Timothy Fields	CTDOT	(860) 594-3217	timothy.fields@ct.gov
Meziane Meziani	CTDOT	(860) 594-3096	meziane.meziani@ct.gov
Robert Moore	CTDOT	(860) 594-2065	robert.moore@ct.gov
Andre St. Germain	AECOM	(860) 263-5837	Andre.St.Germain@aecom.com
Ryan Apanovitch	AECOM	(860) 829-2260	Ryan.Apanovitch@aecom.com
Bea Hunt	AECOM	(212) 973-2946	Beatrice.Hunt@aecom.com
Ellen Newman	AECOM	(978) 905-2583	Ellen.Newman@aecom.com

There were four (4) additional attendees streaming the Microsoft Teams Live Event and one (1) streaming through YouTube Live.

PROJECT PURPOSE AND NEED:

The project is the Rehabilitation of Bridge No. 00571A (Commodore Hull Bridge). Specifically, the project is for installing scour countermeasures at Piers 9 and 10 within the Housatonic River.

PUBLIC INFORMATIONAL MEETING FORMAT:

The project team delivered a recorded presentation to the public and stakeholders of the Cities of Derby and Shelton on January 13, 2022, at 7:00 pm. Attendees had the option of attending the meeting via YouTube or Microsoft Teams live stream, or by calling in and listening by phone.

Following the presentation, a live question and answer session was held. Members of the public and stakeholders submitted questions via email, phone, and through the Teams Q&A window. The project team and Department addressed all comments and questions verbally.

It was noted and reiterated throughout the live Q&A session that Project information can be found on the Project website, and that questions can be sent by calling the phone number or email address below. Attendees were also

encouraged to fill out a survey. Attendees were notified that comments are open for a two-week period until January 27, 2022. A recording of this meeting will be available on the website within five (5) days of this event.

YouTube Recording: <https://www.youtube.com/watch?v=NI5IUR4f3ms>

Email: DOTProject126-176@ct.gov

Phone: (860) 944-1111

Website: <https://portal.ct.gov/DOTSheltonDerby126-176>

Survey: <https://www.surveymonkey.com/r/QGXFVM8>

QUESTIONS AND ANSWERS SESSION:

The following are the questions and comments submitted by the public and stakeholders, along with their respective responses:

1. Will there be any traffic impacts or a detour?

There will not be any traffic impacts as a result of this project. All of the work on the project will occur below the bridge within the river. There will likely be a staging area for Contractor equipment and materials below the bridge on the Derby side, but parking and access to the Derby Greenway will be maintained at all times. This is the same area that was used for the previous project 126-170, which was completed in 2019.

There will be some impacts to recreational boating during construction, as there will be two barges and construction equipment in the river. These impacts will be minor, because construction will be completed during the winter.

2. How is the project funded?

The project is funded with 80% federal funds and 20% state funds. There is no municipal funding.

3. Is the existing bridge safe to use?

Yes, this bridge is safe to use. Every two years the bridge undergoes a hands-on inspection of the superstructure and substructure and is given a rating. The rating scale from best to worst condition goes: Good, Satisfactory, Fair, Poor, Serious, and Critical. This bridge is rated as Satisfactory. The bridge also undergoes an underwater inspection every two years. The underwater inspection reports show a history of scouring, but today it is still far from the critical scour depth. The purpose of this project is to add scour countermeasures around the piers as a preventative measure to prevent the riverbed from scouring further during future major storm events.

4. What happens if the river floods during construction?

The Connecticut Standard Specifications require the Contractor to develop a Flood Contingency Plan. It will be reviewed and approved by the State and AECOM prior to construction. It will include protocols for the protection of the construction site, and for tie-offs and relocation of equipment and materials before and during a flood.

5. Will construction or the final condition countermeasures affect fishing and wildlife?

The countermeasures are not anticipated to significantly impact fishing or wildlife. As part of the project design process, permits will be secured from the Connecticut Department of Energy and Environmental Protection and the United States Army Corps of Engineers. Through this permitting effort coordination will occur with federal and state fish and wildlife programs to avoid and minimize any permanent impacts to the habitat that is currently present on site.

6. Is there any contamination in the soil at the bridge?

As part of an early geotechnical investigative phase of the project, soil borings were obtained and some of the material was retained to test for contaminants. The results of those tests indicated that there were no


significant contaminates of concern but there were low levels of semi-volatile organic compounds. Because of this, specifications will be developed as part of the project design which will govern how the material should be handled. Any material that will need to be disposed of off-site will be stockpiled at a nearby location and tested for contaminates prior to trucking it offsite. Material will be disposed at a facility licensed to handle the level of contamination identified during the on-site testing.

7. What happens if construction is not finished by March 30th, 2024?

As part of the Contractor's responsibilities during construction, they will be required to provide a schedule that conforms with the time of year restrictions for in water work. That schedule will be evaluated throughout construction and if unforeseen delays cause work to extend past March 30th, the Contractor and CTDOT will coordinate with the state and federal fisheries staff to determine what is necessary to mitigate impacts to resources of concern during while allowing the remaining work to be completed.

8. Will construction affect recreational boating or industry that uses the navigation channel?

Given the shallow nature of the waters and the dam right upstream there is not a lot of industry that uses the navigation channel. We do expect some impact to recreational boating, but since construction will take place during the winter, those impacts are expected to be minor. There will be barges in the river throughout the winter. There are two spans that are deep enough for boating, so if one span is partially blocked by a barge, the other will be completely open.

Submitted By: 
Andre St. Germain, P.E.
Project Manager, AECOM

Date: January 13, 2022

Approved By: 
Jessica Carvajal
Project Engineer, CTDOT

Date: January 19, 2022

Attachments:
Public Meeting Notice
PowerPoint Presentation

VIRTUAL PUBLIC INFORMATION MEETING NOTICE

The Department of Transportation (Department) will conduct a Virtual Public Information Meeting concerning the proposed rehabilitation of Bridge No. 00571A (Commodore Hull Bridge) on Thursday, January 13, 2022 at 7:00 p.m. The meeting will be live streamed via Microsoft Teams Live Event and YouTube Live. A Question and Answer (Q&A) session will immediately follow the presentation. The presentation will be recorded, and instructions on how to access the meeting and on how to provide comments or ask questions can be found at the project webpage: <https://portal.ct.gov/DOTSheltonDerby126-176>

The live stream of the formal presentation will begin at 7:00 p.m.

The project is identified as State Project No. 0126-0176.

Bridge No. 00571A (Commodore Hull Bridge) carries Route 8 over the Housatonic River, Riverdale Avenue, Hull Street, and Howe Avenue (Route 110) in the cities of Shelton and Derby. The bridge was originally constructed in 1951 and was reconstructed in 1990, which consisted of widening the substructure and superstructure and replacing the concrete deck. The bridge consists of twelve spans totaling approximately 1,578 feet.

The purpose of the project is to mitigate potential scour risk from stream flow in regard to pier foundation stability. Underwater inspection reports show a history of scour at Pier Nos. 9 and 10 in the river. Pier 9 and Pier 10 were both rated Fair (5) in the 2020 Underwater Inspection Report. A scour prediction evaluation was recently completed and found that scour countermeasures are warranted for protection of the foundation of Piers 9 and 10. The following three scour countermeasure alternatives were investigated:

- Permanent steel sheet pile pier foundation enclosures driven to bedrock around each pier.
- Stone riprap placements around the river bottom around each of the two piers
- Precast concrete interlocking armoring unit placements on the river bottom around each of the two piers.

The recommended scour protection design alternative for Bridge Pier Nos. 9 and 10 are precast concrete interlocking armoring unit placements on the river bottom around each of the two piers.

Right-of-way impacts associated with the proposed project are not anticipated.

Construction is anticipated to begin in the fall of 2023 based on the availability of funding and approval of permits. The estimated construction cost for this project is approximately \$8.3 million. This project is anticipated to be undertaken with 80 percent Federal Funds and 20 percent State funds.

The Virtual Public Information Meeting is being held to provide the public and local community the opportunity to offer comments or ask questions regarding the proposed project. Persons with limited internet access may request that project information be mailed to them by contacting Ms. Jessica Carvajal by email at Jessica.Carvajal@ct.gov or by phone at (860) 594-2774, please allow one week for processing and delivery.

Individuals with limited internet access can listen to the meeting by calling 1-888-566-5916 and entering the Participant Code when prompted: 9977843. Persons with hearing and/or speech disabilities may dial 711 for Telecommunications Relay Services (TRS). The MS Teams Live Event offers closed captioning for the hearing impaired and non-English translation options. A recording of the formal presentation will be posted to YouTube following the event and closed captioning (including non-English translation options) will be available at that time. The recording will also be available in the list of Department Virtual Public Information Meetings here: <https://portal.ct.gov/dot/general/CTDOT-VPIM-Library>

Visit the project webpage for options for Apple users. During the Q&A session and the 14 day comment period that follows the meeting, individuals may leave a question or comment via email (preferred) at DOTProject126-176@ct.gov Individuals may also leave a voicemail question or comment by calling (860) 944-1111, please reference the project in your voicemail.

Language assistance may be requested by contacting the Department's Language Assistance Call Line (860) 594-2109. Requests should be made at least five business days prior to the meeting. Language assistance is provided at no cost to the public and efforts will be made to respond to timely requests for assistance.

State Project No. 126-176

Scour Countermeasures at Bridge No. 00571A,
(Commodore Hull Bridge)
Route 8 over the Housatonic River, Route 110, and local streets
Cities of Shelton and Derby

Virtual Public Information Meeting

Live Event: Thursday, January 13th, 7:00 p.m.

Join us for a Question and Answer session following the formal presentation and send us a question or a comment during the session.

Project Email: DOTProject126-176@ct.gov

Phone: (860) 944-1111 (please leave a voicemail)

Visit the Project Webpage:

<https://portal.ct.gov/DOTSheltonDerby126-176>

A recording of this presentation will be posted after the live event. Closed captioning, including non-English translation options, will be available at that time.

La grabación de esta presentación estará disponible después del evento en YouTube, incluyendo subtítulos y acceso a traducciones en otros idiomas.



Scour Countermeasures at Bridge No. 00571A // State Project No. 126-176



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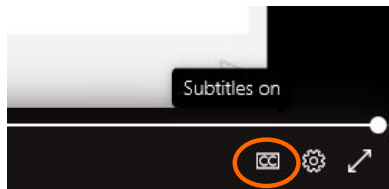
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Captions / Subtitles Instructions (MS Teams Only)

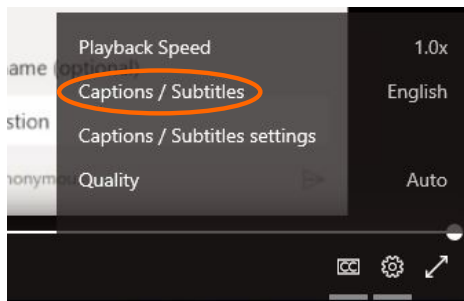
1. Select "CC" at bottom right



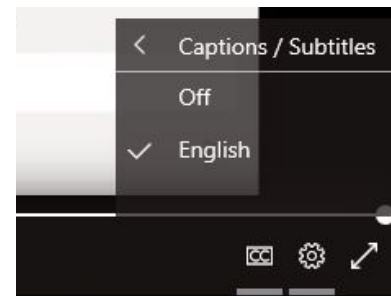
2. Select "Settings" wheel



3. Select "Captions/Subtitles"



4. Select desired language



Available Languages:

- English
- Spanish
- Polish
- Portuguese



Question & Answer Session – Send a Question or Comment via:



by email: DOTProject126-176@ct.gov



by phone: (860) 944-1111 (please leave a voicemail)



by chat: [MS Teams Live Event chat](#)



project webpage: <https://portal.ct.gov/DOTSheltonDerby126-176>

Note: The comment period is open through **January 27, 2022** *MS Teams Chat Function is Only Available During Live Q&A Session.

Scour Countermeasures at Bridge No. 00571A // State Project No. 126-176



Question & Answer Session – MS Teams Live Event Chat:

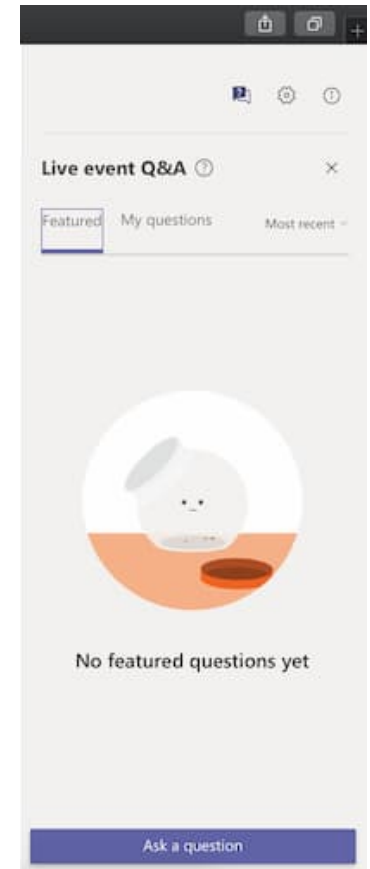
In this format you can ask a question by clicking on the question mark (which is show Q&A). This will bring up your live event Q&A field:

Select the “Ask a question” button at the bottom right screen and field will pop up where you can type your question and then either hit enter or the little arrow button to send.

You can also check the box at the bottom to post your question anonymously if you prefer.

There will be a slight delay, but your question will appear in the “Featured Section” with all other questions asked tonight.

Please note that the MS Teams chat function is only available during live session.



Project Team Introduction



BARTHOLOMEW SWEENEY

CTDOT Division of Bridges /
Division Chief of Bridges



TIMOTHY FIELDS

CTDOT Major Bridge /
Principal Engineer



MICHELLE LYNCH

CTDOT Major Bridge /
Transportation Supervising
Engineer



JESSICA CARVAJAL

CTDOT Major Bridge /
Project Engineer



ANDRE ST. GERMAIN

AECOM
Project Manager



BEATRICE E. HUNT

AECOM
Principal Hydraulic Engineer



RYAN APANOVITCH

AECOM
Environmental Permitting
Lead

Scour Countermeasures at Bridge No. 00571A // State Project No. 126-176



Title VI – Civil Rights

No Person shall, on the basis of race, color or national origin, be excluded from participation or subject to discrimination in the development of this project.

- **Voluntary Post-Meeting Survey:**
 - <https://www.surveymonkey.com/r/QGXFVM8>

- **Title VI Notice to the Public Available:**
 - <https://portal.ct.gov/DOT/Business/Contract-Compliance/Title-VI-Page>



Title VI – Civil Rights (cont...)

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CTDOT Project 126-176
Rehabilitation of the Commodore Hull Bridge (No. 00571A)
Public Information Meeting
01/13/22



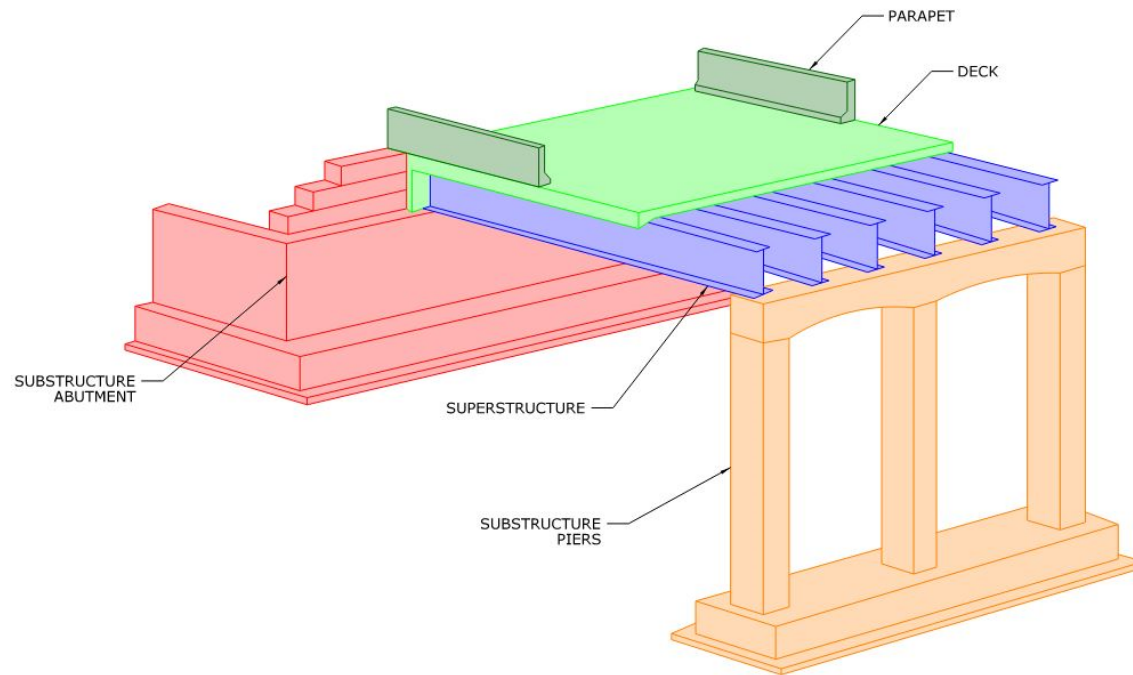
Project Location



Rehabilitation of Bridge No. 00571A // State Project No. 126-176



Typical Bridge Components



Rehabilitation of Bridge No. 00571A // State Project No. 126-176



What is “Scour”?

Scour is the erosion of streambed or bank material due to flowing water.

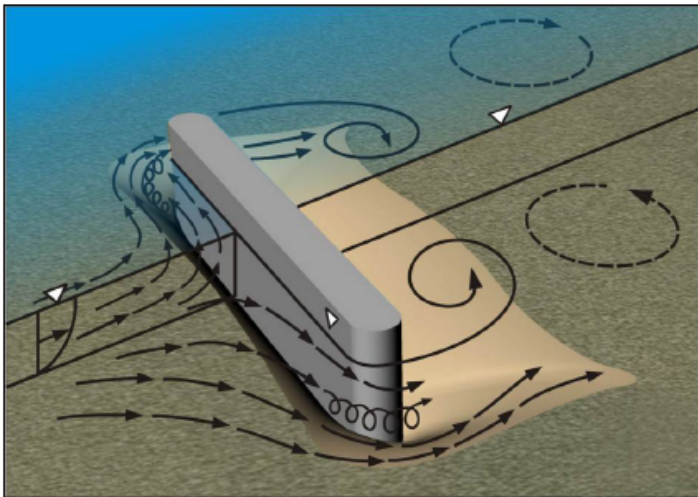
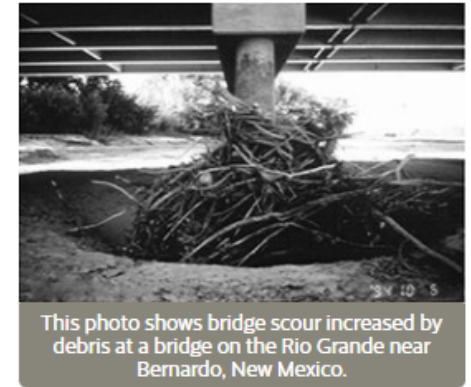


Diagram of scour occurring at a wide pier



Scour occurring in the stream bed around a bridge pier



This photo shows bridge scour increased by debris at a bridge on the Rio Grande near Bernardo, New Mexico.

Local scour at a bridge pier

Source: FHWA HEC-18 Evaluating Scour at Bridges 5th edition, April 2012



Rehabilitation of Bridge No. 00571A // State Project No. 126-176

East Bridge Elevation (Looking West)



Rehabilitation of Bridge No. 00571A // State Project No. 126-176

West Bridge Elevation (Looking East)



Rehabilitation of Bridge No. 00571A // State Project No. 126-176

Pier 9 South Face (Looking North)



Rehabilitation of Bridge No. 00571A // State Project No. 126-176



Pier 10 (in foreground), Pier 9 (in background)



Rehabilitation of Bridge No. 00571A // State Project No. 126-176



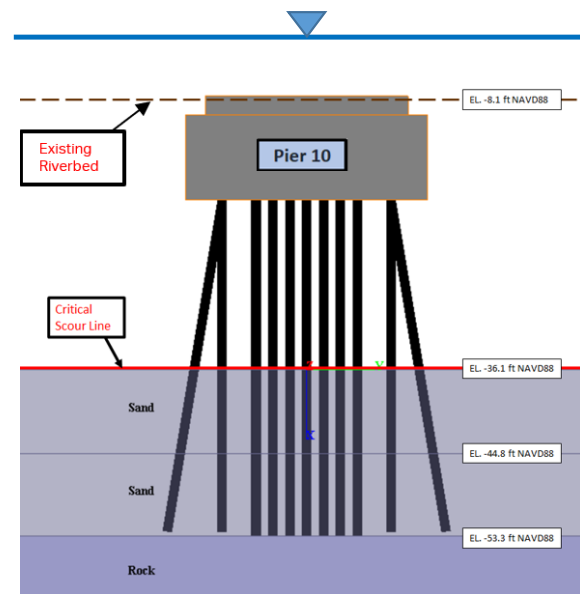
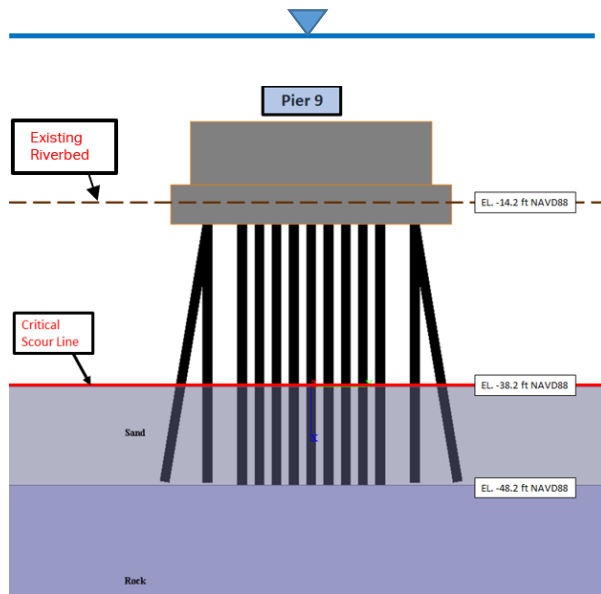
Existing Bridge / Purpose & Need

- Bridge constructed in 1951 and widened in 1990
- 3 main truss spans over river – Piers 9 and 10 within River
- Reinforced concrete piers on steel piles
- Project 126-170 – Steel Repairs, Painting, Install Scour Monitoring System – completed 2019
- Underwater Inspection Report Findings
 - History of Scour at Pier Nos. 9 and 10 in the River
 - 2020 Scour Rating = 5 Fair
 - Pier 9 Upstream Exposed Piles
 - Recent scour prediction evaluation found that scour countermeasures are warranted for protection of the foundations of Piers 9 & 10



Pier Stability Analysis

- Analysis per FHWA guidelines predicts scour to bedrock.
- Critical scour elevations were determined. This is the elevation at which the piers would not perform adequately.
- Therefore, scour countermeasures are warranted.



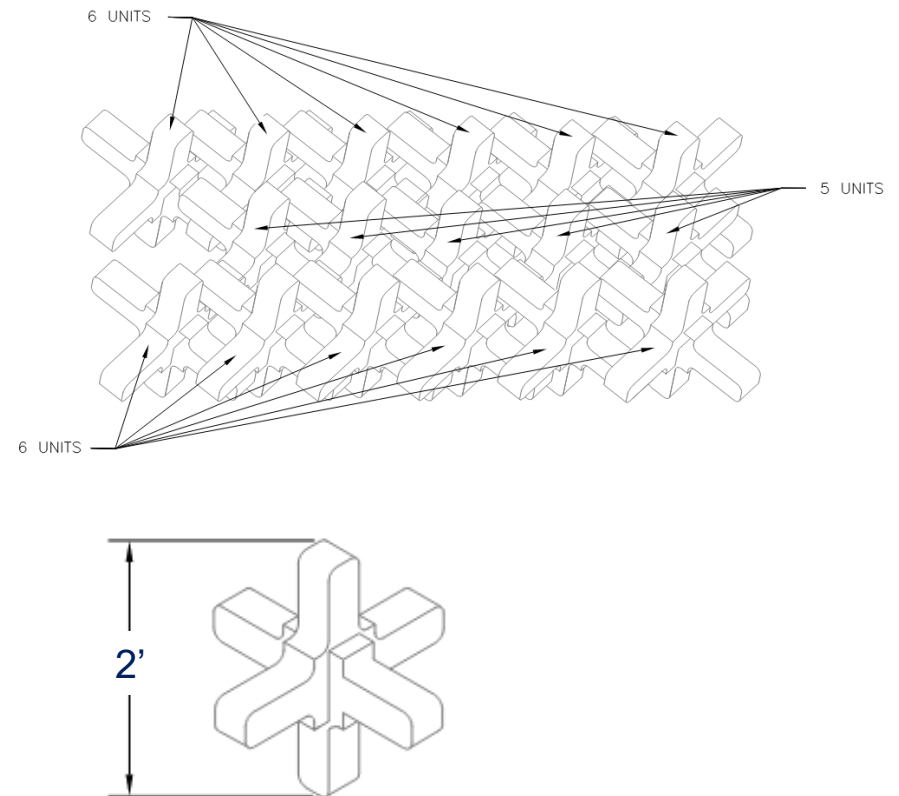
Preferred Alternative: Precast Concrete Armor Units

What are they?

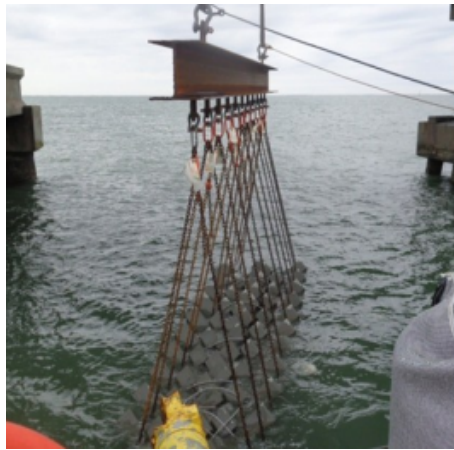
- 6-legged precast concrete jacks
- Installed as 6 x 5 x 6 unit bundles
- Interlocking pattern prevents movement and protects riverbed

Why are they preferred?

- Significantly lower construction cost than other alternatives
- One construction season
- Requires minimal excavation
- Low maintenance
- Protects riverbed and halts ongoing scour around piers



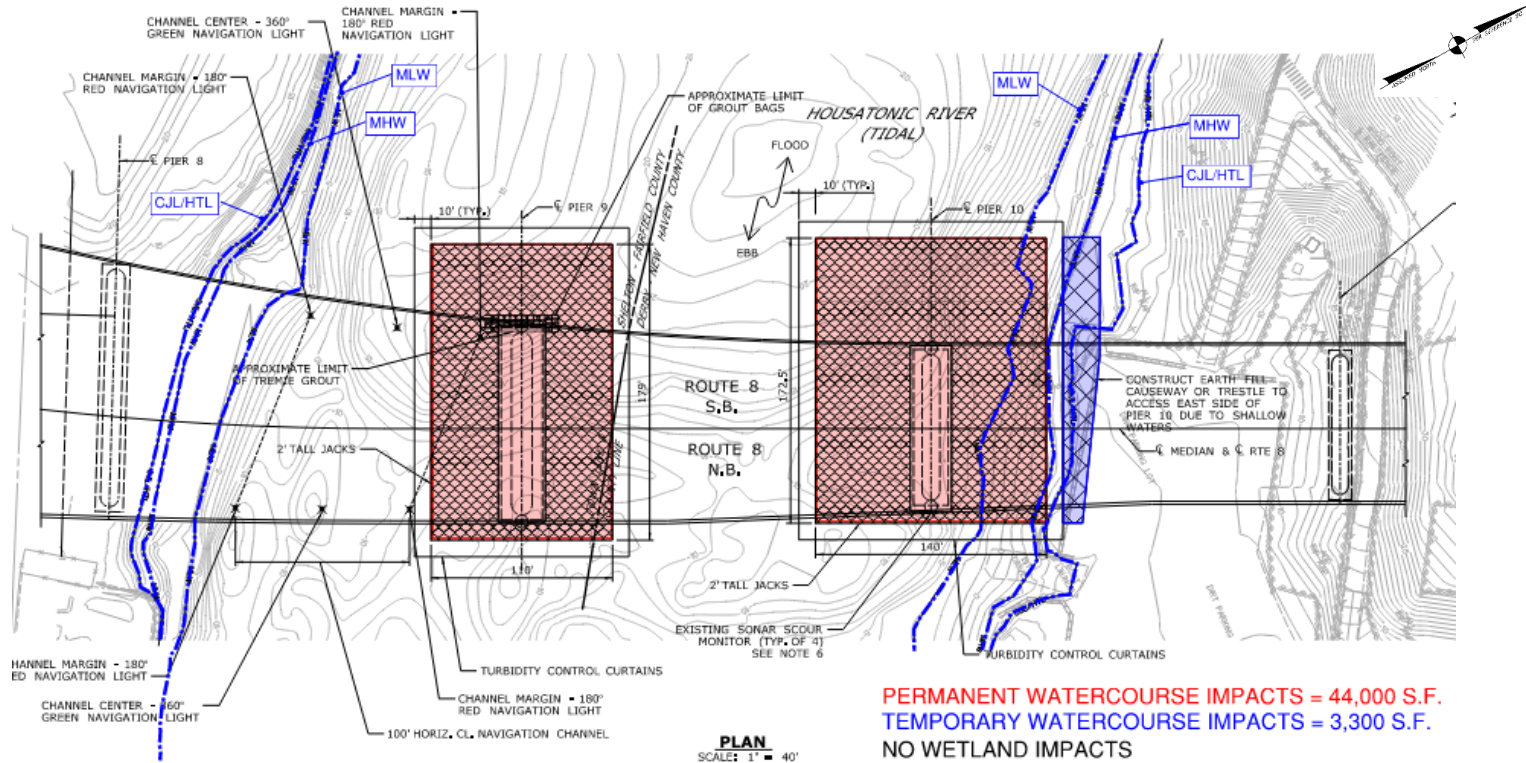
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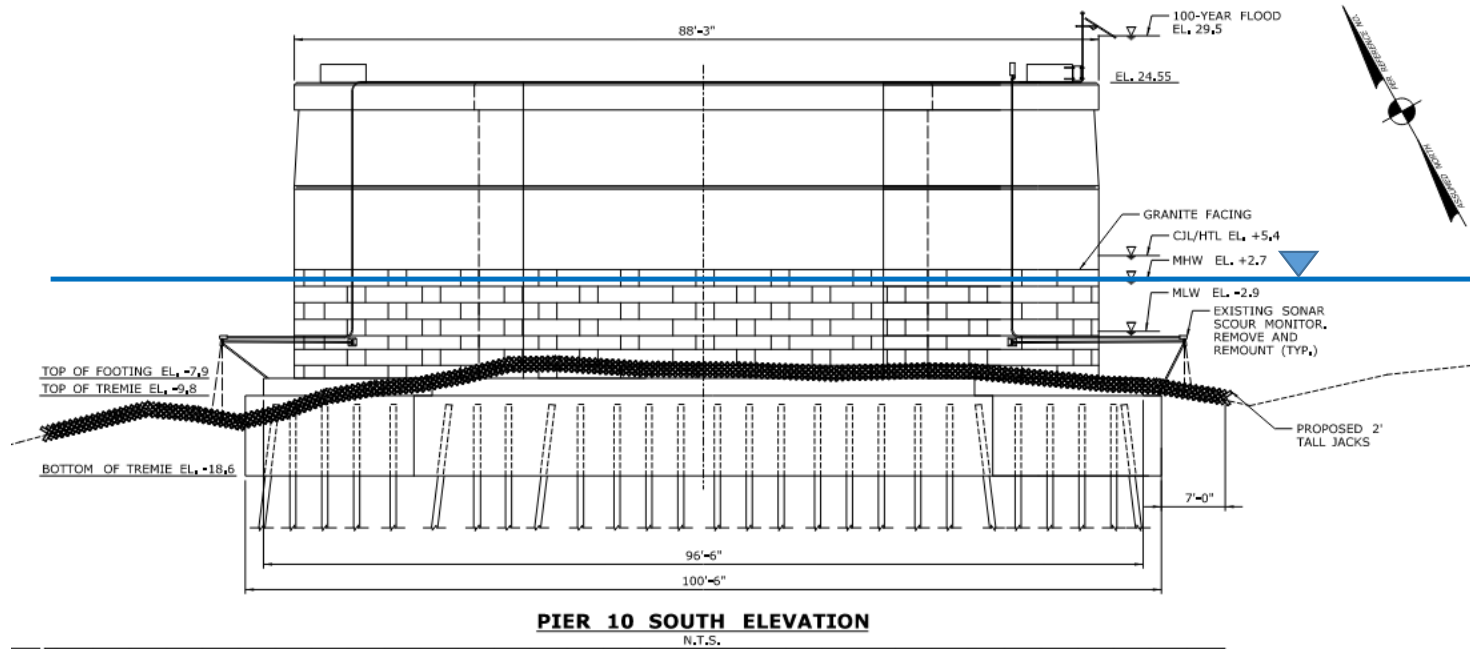
Rehabilitation of Bridge No. 00571A // State Project No. 126-176



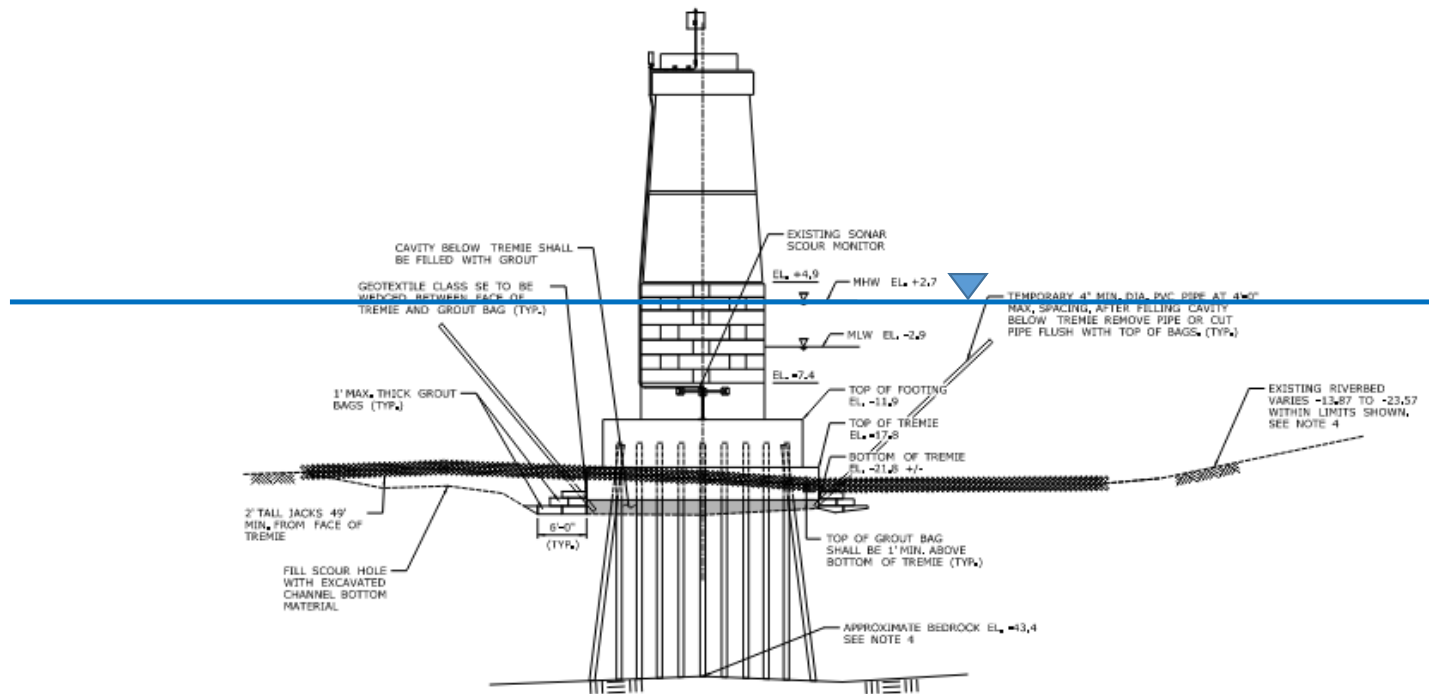
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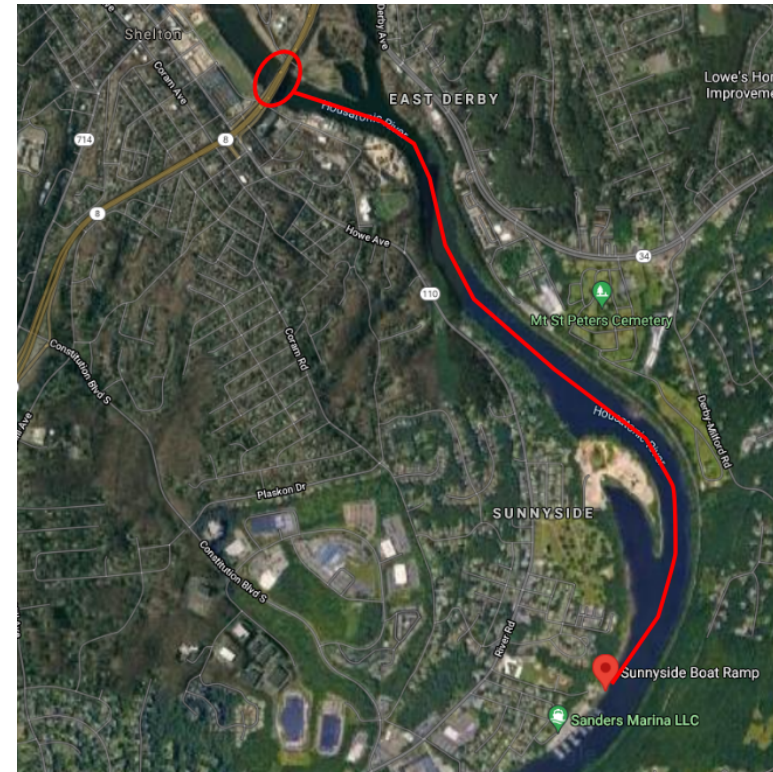
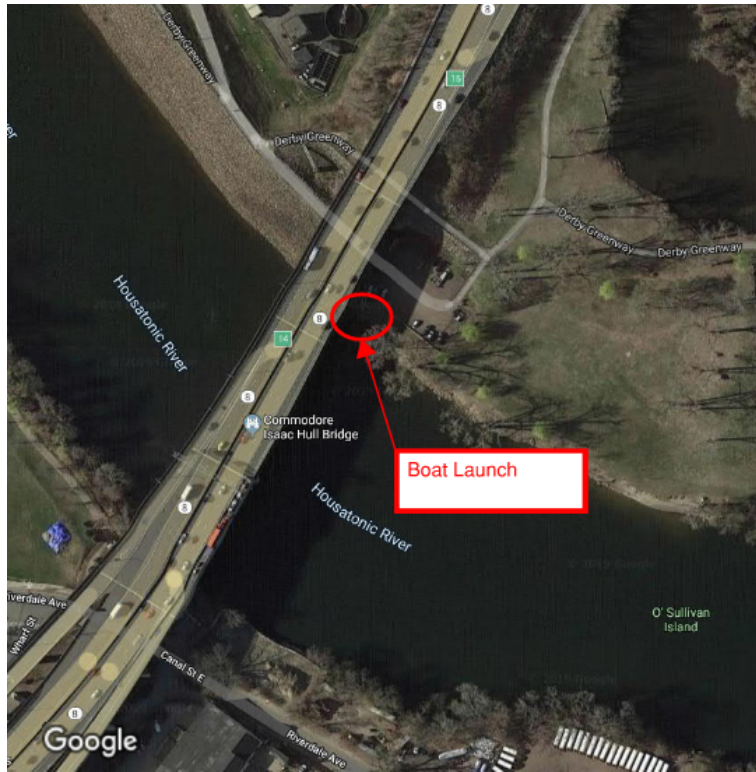
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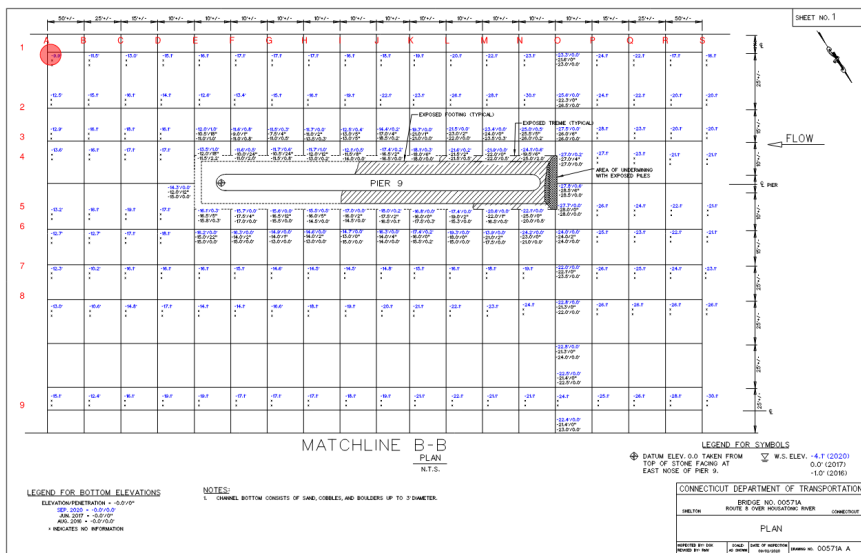
A
17 **PIER 9 WEST VIEW**
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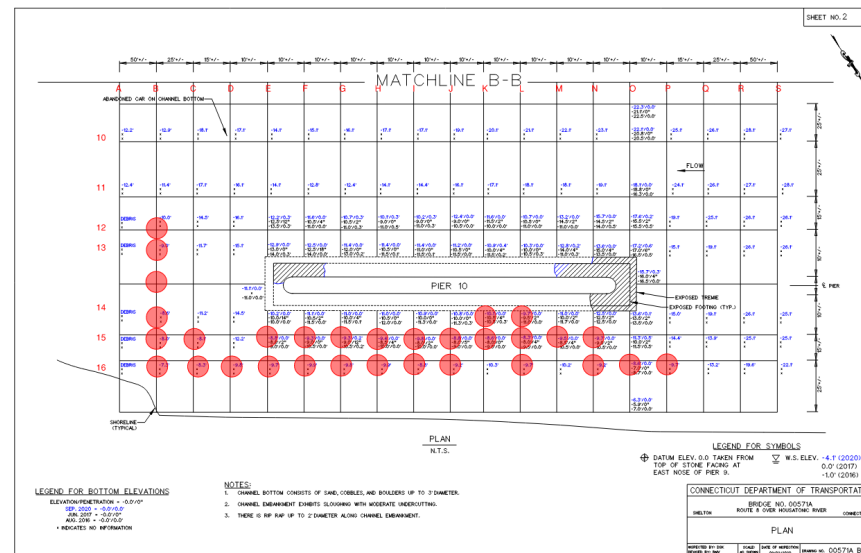
Constructability & Access



Constructability & Access



**Pier 9
(View From Above)**



**Pier 10
(View From Above)**

Source: 2020 UW Inspection Report
 Red markings indicates too shallow for barge use



Constructability & Access

- Traditional barge
- Flexifloat
- Two required:
 - Excavation of 1' Riverbed Material
 - Placement of Precast Units



Rehabilitation of Bridge No. 00571A // State Project No. 126-176



Potential Staging/Stockpile Location - Derby



Potential Staging/Stockpile Location - Shelton



Environmental Permitting

- CTDEEP Permits
 - Structures, Dredge & Fill
 - Tidal 401-Water Quality Certification
 - Flood Management Certification
 - Fisheries – **Unconfined in-stream work limited to October 1st – March 30th**
- USACE Permits
 - Individual Section 404
 - Section 408 Authorization
- USCG Coordination
- CTDEEP Boating Division Coordination



Project Schedule

- Design
 - Preliminary Design: February 2022
 - Final Design: March 2023
 - Advertising: May 2023
- Construction
 - Construction Start: October 1st, 2023
 - Construction Finish: March 30th, 2024
 - Duration: 6 months (3 months each pier)



Construction Cost

- Estimated Construction Cost = \$8M
- Funding:
 - 80% Federal
 - 20% State
 - 0% Municipal



Contact Information

- Project Manager / CTDOT:
 - contact: Michelle Lynch, P.E.
 - email: Michelle.Lynch@ct.gov
 - phone: 860-594-3206
- Project Manager / Designer: AECOM
 - contact: Andre St. Germain, P.E.
 - email: Andre.St.Germain@aecom.com
 - phone: 860-263-5837



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Scour Countermeasures at Bridge No. 00571A // State Project No. 126-176

